

**ARGENTINE NAVY**

NAVAL LOGISTICS MISSION  
IN UNITED STATES OF AMERICA  
(WASHINGTON DC)

**PRIVATE BID  
Nº 08 / 2022  
TERMS, CONDITIONS & FORMS**

**Contracting Entity:** ARGENTINA NAVAL LOGISTICS MISSION IN THE UNITED STATES OF AMERICA – Acquisitions Operational Unit 038/40

**File No:** 200012

**Bidding Purpose:** “Procurement of Sails for the Training Ship Frigate A.R.A. “LIBERTAD” for the Execution of the Training Trip “L” in Baltimore, USA”.

**Bidding Terms and Conditions Pick up - Place and Address:** ARGENTINE NAVAL LOGISTICS MISSION IN THE UNITED STATES OF AMERICA - 630 Indiana Ave. NW, Washington, DC. 20004, USA; website [www.argnavallogistics.us](http://www.argnavallogistics.us) or by electronic mail to [contratacionesmnl@yahoo.com.ar](mailto:contratacionesmnl@yahoo.com.ar)

**Deadline Date and Time:** May 18, 2022 at 08:00 AM to Jun 02, 2022 at 09:30 AM (Eastern Time).

**Bidding Terms and Conditions Questions - Deadline Date and Time:** From 08:00 AM to 2:00 PM (Eastern Time) up until three business days prior to the date and time of the Bid Opening Act.

**Bid Submission Location:** ARGENTINE NAVAL LOGISTICS MISSION IN UNITED STATES OF AMERICA – 630 Indiana Avenue, NW, Washington, DC., 20004, USA: in person, by certified mail or electronic mail to [justbidsmnl@yahoo.com](mailto:justbidsmnl@yahoo.com).

**Bid Submission Deadline and Time:** Bids can be submitted in person, by certified mail, electronic mail at the Argentine Naval Logistics Mission, 630 Indiana Ave. NW, Washington D.C. 20004, **and will be accepted by the contracting entity up to ONE (1) hour before the Bid Opening Act scheduled time.**

**Bid Opening Location:** ARGENTINE NAVAL LOGISTICS MISSION – 630 Indiana Ave. NW, Washington, D.C. 20004, USA.

**Bid Opening Date and Time:** Jun 02, 2022 at 10:30 am (Eastern Time)

**Delivery Port:** FRIGATE A.R.A. “LIBERTAD” - BALTIMORE, USA

**ANNEX I**  
**BIDDING TERMS AND CONDITIONS**

**ARTICLE 1. LEGAL REGULATIONS**

Should an issue not covered by this document arise, the following current regulations in Argentina will apply Decree 1023/2001 and its implementations. These regulations are available at the following websites: [www.argentinacompra.gov.ar](http://www.argentinacompra.gov.ar) and [www.infoleg.gov.ar](http://www.infoleg.gov.ar), they can also be viewed at the Acquisitions Operational Unit 038/40 of the Argentine Naval Logistics Mission in the United States of America, located at 630 Indiana Ave. NW, Washington, DC. 20004, USA.

**ARTICLE 2. BIDDING PURPOSE**

This request for quotation is implemented for the "Procurement of Sails for the Training Ship Frigate A.R.A. "LIBERTAD" for the Execution of the Training Trip "L" in Baltimore, USA".

**ARTICLE 3. TECHNICAL SPECIFICATIONS**

Offers that do not meet the requirements listed in **Annex II and Enclosure 1** shall be dismissed with no rights of claims from the bidder.

**ARTICLE 4. OBTAINING BIDDING TERMS INFORMATION AND DOCUMENTS FOR BIDDING**

Bidding terms and conditions can be viewed and picked up Monday through Friday, from 8:00 am until 12:00 pm (Eastern Time), at the Acquisitions Operational Unit 038/40 of the ARGENTINE NAVAL LOGISTICS MISSION located at 630 Indiana Ave. NW, Washington D.C. 20004, USA, Telephone No. (202) 626-2170. They will also be available at our website: [www.argnavallogistics.us](http://www.argnavallogistics.us) or can be requested to the e-mail address: [contratacionesmnl@yahoo.com.ar](mailto:contratacionesmnl@yahoo.com.ar)

**ARTICLE 5. COMMUNICATIONS**

All communications from interested parties, bidders, or awardees shall be carried out exclusively with the Argentine Naval Logistics Mission.

Inquiries may be done in person at the Mission at 630 Indiana Ave., N.W. Washington D.C. 20004; by e-mail at [contratacionesmnl@yahoo.com.ar](mailto:contratacionesmnl@yahoo.com.ar); by telegram or by certified mail to the Argentine Naval Logistics Mission, located at 630 Indiana Ave. NW, Washington DC. 20004. USA.

**ARTICLE 6. SUBMISSION OF BIDS**

Considering the experience of bidding acts performed in this Argentine Naval Logistics Mission in the United States of America and customary procedures, bids may be submitted in an original format, in a sealed, unmarked envelope, with no signatures, legends or markings of any kind that would allow identification of the bidding company. The file number and the time of the bid opening should be clearly stated in the envelope. Bids must be submitted under one of the following ways:

- a. In person at the Argentine Naval Logistics Mission in the United States, located at 630 Indiana Ave, NW, Washington DC. 20004. USA.
- b. By e-mail at [justbidsmnl@yahoo.com](mailto:justbidsmnl@yahoo.com). This email address will be used exclusively for bid submission, and not for any other means or concerns. The offer may not be modified after the deadline and no alterations will be allowed.

**ARTICLE 7. REQUIREMENTS FOR BID SUBMISSION**

Bids shall be in compliance with the following requirements:

- a. Bids must be completed in the **Request for Quotation Form (Annex III)**.
- b. Regardless of the method of presentation, the bid must be delivered to the office of the contracting entity up to **ONE (1) hour prior to the Opening Act**.
- c. The bid must be submitted in English or Spanish, and in U.S. Dollars.

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- d. In the case of presenting the bid at the Argentine Naval Logistics Mission, the bidder must present a sealed envelope, box or package, indicating the specific bid quoted, and the opening/submittal date.
- e. The specific bid quoted must be clearly stated on the sealed envelope or package.
- f. The corrections or alterations to the documents must be properly acknowledged and justified at the bottom of the bid then signed by the bidder. Otherwise, the altered and/or corrected sections will be considered invalid.
- g. The submission of the bid implies that the bidder acknowledges the terms and conditions and accepts the rules of the bidding act.

### **ARTICLE 8. DOCUMENTS TO BE SUBMITTED WITH THE QUOTATION**

- a. Complete the attached quotation form (**Annex III**) **all pages signed**, with the corresponding economic offer, as set out in Article 7 of this document.
- b. If requested by the end user, the bidder must provide technical literature and manufacture specifications of the material offered. In the case that this request is made after the opening date of the bid, the supplier must comply within 72 hours from the first business day after the notification, otherwise, the offer will be voided.
- c. All technical documents required in the technical specifications (**Annex II and Enclosure 1**).
- d. Bids that are not signed by authorized company personnel must have the corresponding documentation attached stating that the bidder is a legal representative for all the processes of the present bidding act.
- e. The bidder should provide a commercial/business address in order to receive the notifications sent by the buyer. Nevertheless, notifications can be made through other channels of communications, such as e-mail, fax, etc.
- f. **The bidder must present a signed Affidavit along with the bid, expressing in detail that the U.S. dollar amount quoted includes the provision of all material indicated in Annex II, Enclosure 1 and Annex III. Should the bidder not comply with the delivery of any of those material, the corresponding penalty described in Article 36.c below would apply.**

**The Affidavit above is requested due to the fact that the vessel port stay in this port is considerably short and these material are necessary for the ship to fulfill its scheduled itinerary.**

### **ARTICLE 9. BID QUOTATION CONTENT**

The bid quotation form should include:

- a. The unit price in numbers, expressed in U.S. dollars, always referring to the unit of measure stated in the bid invitation, and the **total amount of the proposal, in letters and numbers**.
- b. In the event that the total amount quoted for each line does not correspond to the unit price, the latter will be taken exclusively as the quoted price. The quote must include brand, model, manufacturing year, National Stock Number (NSN) Part Number (when applicable), and quality standards that the items comply with.
- c. The quotation bid shall include handling and freight charges required for delivery to the shipping office or desired location requested.
- d. A discount on the price will be accepted, for the total of lines, or for a group of lines, based on the bidding quote.
- e. Alternative offers will be accepted as an option with its terms and documents, always based on the Terms and Conditions that govern the present contract.
- f. In case a mistake is encountered, either on a line or in the total amount of the quote, the unit price will be considered valid.
- g. The quoted price will be considered unique, fixed and invariable during the selection of the bidding process as well as for the contracted period.

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### **ARTICLE 10. BID SUBMISSION**

Regardless of the format of bid submission, all offers received will be securely kept in the Mission's Accounting and Finance Department until the day and time of the bid opening.

### **ARTICLE 11. HONORING OFFER TERMS**

The bidder should honor the offer up to SIXTY (60) calendar days starting from the Bid Opening date. If TEN (10) calendar days prior to the mentioned deadline the bidder does not notify in writing that he/she is not willing to renew the offer, it will automatically be considered renewed for a term equal to SIXTY (60) calendar days and so on. The bidder can state as well as manifest in the quote that he will not renew the offer, or that he will maintain it for a certain quantity of periods or days.

### **ARTICLE 12. MATERIAL**

If requested in Annex II and Enclosure 1 to Annex II, all material will be delivered with its appropriate form or equivalent (Example: for aviation spares "FAA Form-8130").

### **ARTICLE 13. GUARANTEE OF MATERIAL**

The offered and supplied material must be guaranteed by the successful bidder or manufacturer.

### **ARTICLE 14. EXPLANATIONS PRIOR TO BID OPENING**

Before the beginning of the bid opening, the interested parties may request from the contracting entity's office all additional information and explanations deemed necessary up to (3) THREE BUSINESS DAYS prior to the bid opening. This exchange of information will be allowed and the requests satisfied as long as the equal bases are not altered.

The Contracting Entity reserves the right to modify the present document by means of an explanatory or modifying notice up to TWENTY-FOUR (24) business hours before the bid submission.

### **ARTICLE 15. BIDS OPENING**

Bids opening will start at the place, day and hour assigned, opening minutes will be recorded.

- a. All interested parties may be present at the time of the opening.
- b. Offers will be accepted by the contracting entity up to ONE (1) hour prior to the opening act, hence no late offers will be accepted under any circumstances, even if the opening has not yet started, as stated in Articles 6 & 7 of the terms.
- c. If the scheduled date for the bids opening falls on a non-working day, the opening will take place the following business day at the originally scheduled time.

### **ARTICLE 16. BID REJECTION**

Situations that will cause rejections:

- a. If the bid is not signed by the bidder.
- b. If it is written in pencil or any other mode that allows alteration.
- c. If the bid is offered with conditions, that will deem them invalid.
- d. If it has scratches or corrections in fundamental sections such as "price", "quantity", "maintenance time", "shipping time", or any other section of the contract that has not been properly acknowledged.
- e. If it differs from the Terms and Conditions of the regulations that govern this bid, or if it impedes the exact comparison with all other offers.
- f. If the price quoted is not accurate or unreasonable.

### **ARTICLE 17. TIE BREAK BIDS**

The affected bidders will be invited to submit a price negotiation in writing within a deadline set accordingly. If a tie still persists a public drawing will take place at a date and time fixed.

### **ARTICLE 18. GUARANTEE**

According to the local usage customs and in accordance with Article 31 of the Decree Nr. 1023/01, the awardee will not be required to submit and/or constitute a warranty.

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Notwithstanding the above, all bidders and awarded contractors are obligated to deposit the guarantee amount requested by the contracting entity, without any right to make a claim before the payment.

**ARTICLE 19. SUBMISSION OF COUNTER GUARANTEES**

Not applicable.

**ARTICLE 20. EVALUATION OF BIDS CRITERIA**

Among the offers received and admitted, the award will be issued to the most convenient offer as stipulated by the Contracting Entity, this being the lowest price quoted per line, the quality and/or services complying with the conditions, Commercial history in contracting with the Argentine Naval Logistics Mission and requirements established by the Terms and Conditions Sheet and its Annexes.

**ARTICLE 21. CONTRACT CLOSING**

According to article 20 of the Decree 1023/2001, the closing of the contract will take place with the notification, and acceptance of the purchase order by the Awardee, which will be issued by the Contracting Entity within TEN (10) business days from the award notification date.

**ARTICLE 22. CANCELLATION OF BIDS**

Before the closing of the contract, the contracting entity reserves the right to cancel the call for a bid. In this case, the bidders or interested parties will not have the right to demand any compensation or indemnification.

**ARTICLE 23. DELIVERY TERMS**

The provision of the material will be made during the stay of the frigate A.R.A. "LIBERTAD" in the port of Baltimore – USA, as specified in Annex II, Enclosure 1 and Annex III.

**ARTICLE 24. FREIGHT CHARGES**

The cost of freight, licenses, customs expenses, port permit, handling, agency transportation, and any other expense must be included in the unit value of the item quoted for each line.

All those offers that quote these costs separately from the unit value of the item will not be considered valid by this Logistic Naval Mission.

**ARTICLE 25. DELIVERY LOCATION**

Material delivery must take place on board Frigate A.R.A. "Libertad" during her stay at the port of Baltimore, United States of America, between July 8 thru 11, 2022.

**ARTICLE 26. CONTRACT COMPLIANCE**

The Awardee will comply with his/her obligations according to the procedures, terms, location and specifications established within the contract and/or purchase order. The corresponding terms will be effective from the first business day immediately following the date of acceptance of the purchase order.

**ARTICLE 27. REQUEST FOR EXTENSION OF DELIVERY TIME**

The request for an extension of delivery other than the stipulated in the purchase order must be submitted before the deadline explaining the reasons for the delay. The contracting entity will accept the approval of new delivery dates only when justified causes exist, although applicable penalties will still apply.

**ARTICLE 28. UNFORESEEN CASES**

In the case an unforeseen situation comes up which impedes the fulfillment of the contract by the awardee, he/she must notify the Contracting Entity within TEN (10) days of the promised delivery time. After this period, no unforeseen or accidental situation can be invoked.

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### **ARTICLE 29. CONFORMITY RECEPTION.**

The final reception will be provided within ten (10) business days. This period will start after the first business day of the delivery date or the expiration date indicated in the Terms and Conditions of the purchase order, according to Annex II, Article 6.

### **ARTICLE 30. INVOICES**

All invoices must be acknowledged by Training ship Frigate "Libertad" personnel and submitted to this Naval Logistics Mission located at 630 Indiana Ave. N.W. Washington, DC. 20004 or by mail to the indicated addresses, which will start the period set for payment: [contratacionesmnl@yahoo.com.ar](mailto:contratacionesmnl@yahoo.com.ar) , and [ruben.bolognesi@argnavy.org](mailto:ruben.bolognesi@argnavy.org),

### **ARTICLE 31. INQUIRIES**

All inquiries related to conditions and deadlines, purchase orders and/or billing should be addressed directly with this contracting agency. The Naval Logistics Mission in the United States of America will not be responsible for information provided by different entities.

### **ARTICLE 32. PAYMENT TERMS**

Payment terms established in the purchase order will take place for **NINETY (90)** calendar days depending on the availability of funds to pay to the suppliers. The payment process will start, once the invoice is submitted.

### **ARTICLE 33. PAYMENT IN ADVANCE**

Not applicable

### **ARTICLE 34. PAYMENT METHODS**

Payment will be made by electronic bank transfer issued by the National Central Bank of Argentina (Banco Central de la Nación Argentina) to the bank account indicated by the grantee once he/she is notified of the award. He/she must provide our Accounting Department the necessary banking information. Payment will be made in US Dollars for the amount indicated on the purchase order and / or invoice. In no case, monetary update or price indexation will apply.

### **ARTICLE 35. END USER CERTIFICATE**

In the corresponding cases, the successful bidder must require this Contracting Entity to sign the end user certificate, this being a condition for the approval of the authorization by the Department of State or Department of Commerce.

### **ARTICLE 36. PENALTIES**

If applicable, the following penalties will be issued to the bidders/awardees.

- a. **Loss of offer period guarantee:** If the bidder expresses to not maintain its offer outside the deadline dates set for making such a statement, or withdraws its offer without meeting the offer maintenance deadlines.
- b. **Loss of contract period guarantee:** The awardee breaches the established contract before or after the delivery time and its extensions, as well as the demands to comply period issued by the Frigate A.R.A. "Libertad" reception committee. This penalty applies to those situations where the material is not delivered.
- c. **Delivery delay:** A penalty of 0.05% per day will apply to the total amount of those materials delivered after the contract established delivery time.

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## ANNEX II SPECIAL CONDITIONS

### 1. SUBJECT

To establish the special conditions for the "Procurement of Sails for the Training Ship Frigate A.R.A. "LIBERTAD" for the Execution of the Training Trip "L" in Baltimore, USA".

### 2. MATERIAL TO PROCURE

ITEM	NS N	P/N	DESCRIPTION	UM	QUANTIT Y	QUALIT Y
1	-	-	<b>Lower Top Stay Sail</b> (Luff: 23,00 mts; Foot: 9,90 mts; Leech: 17,50 mts; Diagonal/Radius: 7,00 mts).	EA	1	FN
2	-	-	<b>Upper Top Stay Sail</b> (Luff: 20,00 mts; Foot: 10,20 mts; Leech: 15,00 mts; Diagonal/Radius: 7,50 mts).	EA	1	FN
3	-	-	<b>Top Gallan Stay Sail</b> (Luff: 21,30 mts; Foot: 10,60 mts; Leech: 14,50 mts; Diagonal/Radius: 6,50 mts).	EA	1	FN
4	-	-	<b>Aft Gaff Sail</b> (Luff: 7,60 mts; Foot: 13,50 mts; Leech: 13,00 mts; Side: 9,00 mts, Diagonal/Radius: 15,50 – 14,25 mts).	EA	1	FN
5	-	-	<b>Lower Top Sail</b> (Luff: 23,00 mts; Foot: 24,70 mts; Side 5,90 mts).	EA	1	FN
6	-	-	<b>Lower Mizzen Top Stay Sail</b> (Luff: 23,00 mts; Foot: 9,90 mts; Leech: 17,50 mts; Diagonal/Radius: 7,00 mts).	EA	1	FN
7	-	-	<b>Mizzen Gallan Stay Sail</b> (Luff: 21,30 mts; Foot: 10,60 mts; Leech: 14,50 mts; Diagonal/Radius: 6,50 mts).	EA	1	FN

### 3. TOLERANCES

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Tolerances will not be accepted.

### **4. DELIVERY PERIOD**

The provision of the material will be carried out during the stay of the Frigate A.R.A. "LIBERTAD" specified in this Annex II and Attachment Nro. 1. The day and time of the provision of the material will be confirmed by the Authorities of the Training ship Frigate A.R.A. "LIBERTAD". For operational reasons, with prior notice from the Applicant, the possibility that the provision of the material is made up to 72 hours before the scheduled arrival at port will be considered.

### **5. INSPECTION AND RECEPTION**

- a) The provisional reception will be made by the Argentine Navy, being in charge of the Training Ship Frigate A.R.A. "Libertad", preparing the corresponding Act.
- b) The final reception will be executed by Frigate A.R.A. "Libertad" personnel, drafting the appropriate Final Reception Act once the services have taken place.

### **6. DOCUMENTATION**

The supplier must present the following documentation once the material are delivered:

- a) Purchase Order.
- b) Invoice containing the following information:
  - The invoice must be made payable to:
  - **ARMADA ARGENTINA**
  - **Benjamín Juan Lavaisse 1195 – C.P. (C1107ALJ) – CABA - Argentina**
- c) Packing Slip / Details of the goods
- d) Payment Terms
- e) Delivery Date
- f) File N°: Req. 2 00 012
- g) Delivery receipt/acknowledgement signed by Frigate A.R.A. "Libertad" personnel.

### **7. TECHNICAL AND ADMINISTRATIVE SUPPORT**

For technical and administrative questions please send your inquiries to [contratacionesmnl@yahoo.com.ar](mailto:contratacionesmnl@yahoo.com.ar)

**ENCLOSURE 1 to ANNEX II**  
**TRAINING SHIP FRIGATE A.R.A. "LIBERTAD"**  
**SAILS REQUIREMENTS**  
**BALTIMORE (UNITED STATE OF AMERICA)**

N°	Description	Unit of Measure	Quantity
1	<b>Lower Top Stay Sail</b> (Luff: 23,00 mts; Foot: 9,90 mts; Leech: 17,50 mts; Diagonal/Radius: 7,00 mts).	EA	1
2	<b>Upper Top Stay Sail</b> (Luff: 20,00 mts; Foot: 10,20 mts; Leech: 15,00 mts; Diagonal/Radius: 7,50 mts).	EA	1
3	<b>Top Gallan Stay Sail</b> (Luff: 21,30 mts; Foot: 10,60 mts; Leech: 14,50 mts; Diagonal/Radius: 6,50 mts).	EA	1
4	<b>Aft Gaff Sail</b> (Luff: 7,60 mts; Foot: 13,50 mts; Leech: 13,00 mts; Side: 9,00 mts, Diagonal/Radius: 15,50 – 14,25 mts).	EA	1
5	<b>Lower Top Sail</b> (Luff: 23,00 mts; Foot: 24,70 mts; Side 5,90 mts).	EA	1
6	<b>Lower Mizzen Top Stay Sail</b> (Luff: 23,00 mts; Foot: 9,90 mts; Leech: 17,50 mts; Diagonal/Radius: 7,00 mts).	EA	1
7	<b>Mizzen Gallan Stay Sail</b> (Luff: 21,30 mts; Foot: 10,60 mts; Leech: 14,50 mts; Diagonal/Radius: 6,50 mts).	EA	1

**1. GENERAL TECHNICAL SPECIFICATIONS FOR ALL ITEMS**

**1.1. GENERAL AND OPERATIONAL SPECIFICATIONS**

1.1.1. All sails manufactured for the Frigate A.R.A. "LIBERTAD" must be built to the exact measures that the vessel possesses, and be able to withstand the treatment that they normally receive during its navigation as part of the vessel's "training ship" mission, for which they may not have any limitations for any type sail maneuvering.

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1.1.2. Usually, this vessel will sail through all waters of the world, where it is expected to cross certain areas where bad weather and strong winds are very high.

1.1.3. Mixed propulsion (sail and motor) is the normal navigation condition.

1.1.4. With headwinds, the vessel can sail with a fairly good centerboard angle, in the order of 30°/35°. With tailwinds, a fair to strong intensity wind is needed to achieve an acceptable speed.

1.1.5. Due to the above, it is required that the sails be extremely resistant since they will be used for a long time, in different wind conditions and operated by personnel who are in training period.

1.1.6. Likewise, the sails provided must ensure the current operating conditions of the Training Ship, allowing it to reach, at a minimum, the speeds recorded according to its records, both in mixed navigation (sail and motor) and pure sail.

### **1.2. SAILS DESIGNS AND SHAPES (FOR SQUARE SAILS, STAYS AND GAB)**

The sails design with panels oriented parallel to the leech/side is the one currently in use, this condition is not restrictive, since there are renowned sail makers that apply all their technology to optimize the sail performance.

The sails to be delivered, according to these specifications, must generally be flat with a maximum deflection in the bag approximately 1/3 of the horizontal width from the luff. The bidding sail maker must indicate the techniques and technology used in the final design. As an indication, without limiting the sail maker in the application of its own technology, the number of sail pockets is established as luff curvature arrow, the sail extended in a horizontal plane. Said value will be in the order of 2.4% of the length of the luff, modified by several factors such as the type of sail, curvature of the leech, etc.

The use of the negative curvature of the leech of all the sails will be left to the technical criteria of the sail maker, compatible with the principle of optimizing their general performance.

It is hereby stated that in all cases the sail maker will be responsible for the performance, final quality and ability to comply with the stated working conditions of the sails it provides, whose design has been modified by it.

### **1.3. FABRICS**

The sails material must be woven fabric with high-tenacity continuous filament polyester yarn (dacron, terylene, tergal, tetorón: brands depending on the country of origin), with heavy calendering and Taffeta weave. They must have an ultraviolet protection and be white.

These materials finish must be of the stabilized type, which protects them from the possible hardening caused by the exposure of the fabric to ultraviolet rays, thus reducing the difficulties of maneuvering the wing.

Fabrics must meet the following minimum properties:

- Fabrics for the Sails of the Normal Rigging: (Sails Bungee Stay, Over Bungee Stay, Gaff, Overhead stay)

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Material	Dacron – Dupont Certified
Weight, Minimum	500 grams / square meter
Ligament	Tafettan
Tension Resistance, minimum Warped Weft	340 kgs 430 kgs

Cape Rigging Sails Fabric: (Topsail Forestay Sails, Over Mesana Forestay, Low Topsail)

Material	Dacron – Dupont Certified
Weight, Minimum	560 to 620 grams / square meter
Ligament	Tafettan
Tension Resistance, minimum Warped Weft	410 kgs 380 kgs

The sail maker must describe in detail the offered fabrics, providing data such as brand, type of yarn, batch, particular characteristics and mechanical properties. Stress-elongation graphs are required in the direction of the warp (0°), of the weft (90°), of the bisector (45°) and the standard applied to obtain them, in order to carry out subsequent checks.

### 1.4. CUTS

All jibs and stays have a central rib cut known as "English", which facilitates the distribution of efforts between the 3 cuffs. The square sails have vertical cuts and the gaff has a horizontal cut.

All sails will be made with the following cuts:

Vertical cut: all square sails.

Radial cut: cut with central rib or English cut and will be all stays.

### 1.5. SEAMS AND THREAD

The joints between the panels that make up the sail will contain no less than 3 lines of zig-zag stitching (Zig-Zag Triple stich) made with high-tenacity polyester thread, and from ultraviolet radiation. As a guide, the wire may be Dabond V 138 type or similar. The minimum tensile strength of the joint must be 320 Kg.

The seams must be protected with a special resin that provides resistance to ultraviolet radiation and abrasion. The seams within a distance of 1 meter from the leech must be additionally protected with adhesive Dacron tapes.

The panels overlap will be carried out to the limit with 50 mm overlaps and 3 zig-zag seams by straight stitching with 4 stitches per seam, avoiding the possible risk of unraveling. The

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seam width should be 10mm and 10mm apart from each other. Likewise, all these overlaps of cloth will be pre-glued with 18 mm double-sided adhesive tape. wide, which achieves a adhesion in the seams much higher than the normal one made with thread.

The thread used for these seams is continuous-spun three-core polyester. Said seams will be protected by fabric bands 5 cm wide, so that they are not visible, and in case of friction, it is the band that suffers, being able to replace it in case of tear, without the need to repair the seam.

The thread used is treated with virgin bee wax plus ultraviolet protection, since if the thread is affected by the sun's rays, the existence of the seam is in danger.

### **1.6 RELING**

Sail ropes must meet at least the following characteristics

CHARACTERISTICS	REQUIREMENT
Material	Polyester
Thickness	16 mm
Tension Resistance	4,600 kgs

All sails on their head ropes must contain double 200mm fabric folded, covering the rope (the rope will be covered by two layers of fabric). This fabric should be sewn with three zig-zag stitches.

### **1.7. EYELET-HOLE AND CRINGLE**

The union with the sails is made through stainless steel eyelets, protected with hard plastic rings of the "ring" type, a material equivalent to steel and which is provided with small metal spikes on its lower part (it prevents the tearing of the themselves going into the fabric), thus providing a strong consistency to the shooting point. The Eyelet-Hole will have the following characteristics:

Amura and Halyard Cuff Eyelets

CHARACTERISTICS	REQUIREMENT
Material	Stainless Steel
Steel Type	AISI 316
Interior Diameter	55 mm
Design	Rutgerson

CHARACTERISTICS	REQUIREMENT
Material	Stainless Steel
Steel Type	AISI 316
Interior Diameter	35 mm

CHARACTERISTICS	REQUIREMENT
Material	Stainless Steel
Steel Type	AISI 316

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Interior Diameter	35 to 45 mm
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The pulleys of the sail grips must be made of stainless steel, in one piece, with a finish that does not give off points that could cause cuts in the sail.

### 1.8. RINGS

The characteristics will be the following:

#### Clews Rings

CHARACTERISTICS	REQUIREMENT
Material	Steel 316
Thickness	19 mm
Interior Diameter	140 to 190 mm. May have a cross to achieve resistance
Finishing	Polished & Bright/Shinny

#### Briole Guide Rings

CHARACTERISTICS	REQUIREMENT
Material	Steel 306
Thickness	6 mm
Interior Diameter	50 to 60 mm.
Finishing	Polished & Bright/Shinny

### 1.9. REINFORCEMENT

Reinforcements are the areas where the points or chafing areas are located. The material used in the reinforcements is Dacron, its name is Patch material and its interweaving is of the anti-tear type that prevents cracks due to tension with a grammage of greater resistance than that of the sail.

The swinging areas against the mast in the square sails and on their aft face, will have a 90 cm wide strip from the luff to the foot of the same construction material as the sails, as well as a 60 cm wide reinforcement, throughout the length of their headlines.

An interchangeable sacrificial cloth will be placed on the mainsails to protect them from friction and grease stains from the standing rigging. This reinforcement will be superimposed on the forward face of the sail, diagonally from the center of the luff head rope down towards both sides, becoming firm at the foot where it is practically taken up by the clew, with a width of no less than 90 cm.

Blade sails must have a 20 cm wide reinforcement along the entire length of their ropes.

The cuffs will be reinforced with woven tape, protective leather and sacrificial leather, seams with braided paraffin-based polyester threads and nickel steel eyelets. Likewise, they must be provided with trevira strips that start from the eyelet and die like spokes at a distance of 50 cm from it. In this way, in the unlikely event that it were to tear, it would still be firmly attached to the vela, allowing a fortune to be fixed without damaging it. The layers of the

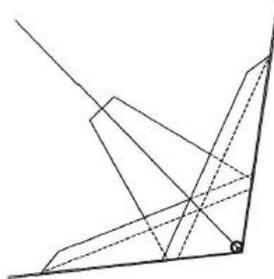
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cuffs will be laminated and adhered with self-adhesive tape, which prevents displacement in the preparation, to eliminate wrinkles in the cloth as much as possible. The radial system of eyelets allows a distribution of loads in the cuffs and protection in the areas of friction with the rigging and the rigging. The latter is of vital importance for the conservation of velas. The size of the fists will be approximately three square meters, varying according to each vela and fist as detailed below:

### **Clews**

The center of the cuff reinforcement should contain 6 layers of fabric distributed as shown in the figure below.



At its end it must have a ring with the characteristics described in point 1.8, attached to 3 or 4 35 mm eyelets using 50mm tape. The 35mm eyelets are reinforced with 50mm tape with a length of 400 mm.



In addition, it must be protected by a sacrificial leather, which in its design can be as shown



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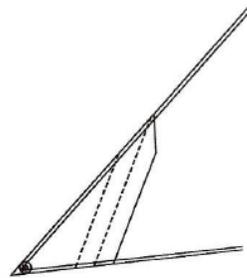
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in the figures.

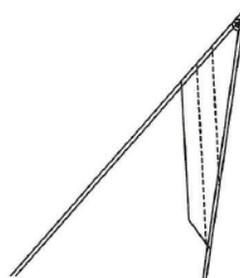
Note: This grip must be made to ensure at least a load of 5 tons of force.

Tack Cuffs and Halyard Sails Blades:

The center of the cuff reinforcement must contain 3 layers of fabric distributed as shown in the following figure:

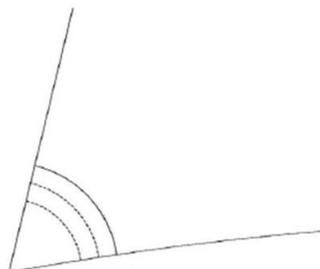


Amura velas cuchillas

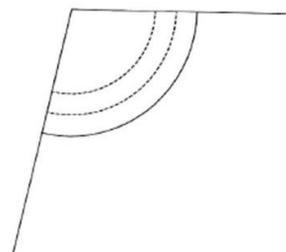


Driza velas cuchillas

Clews and handles stable sails

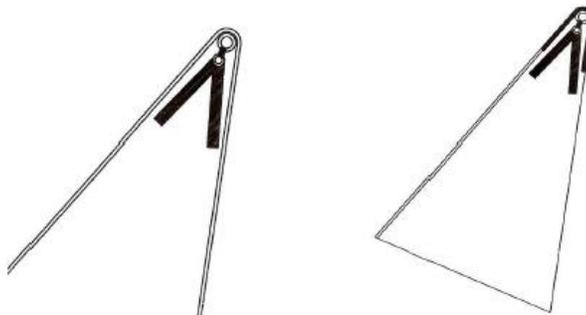


Clews Stable Sails



Handles Stable Sails

At its end it must have an eyelet with the characteristics described in point 1.7 (Amura and Halyard Cuff Eyelets), attached to a 35 mm eyelet. using 25mm tape. In turn, the 35 mm eyelet is reinforced with 50 mm tape. of a length of 400 mm. In addition, it must be protected by a sacrificial leather, which can be as shown in the following figure:



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## **Note:**

This fist must be made to ensure at least a load of 4 tons of force

## **1.10. SAIL MARKS:**

All sails must contain on their grips the name of the sail, name of the grip and the year of manufacture. The height of the letter must be between 2 to 3 cms.

## **2. WARRANTY AND AFTER-SALES SERVICE**

The bidding companies must issue a guarantee certificate of no less than FOUR (4) years from the delivery of the sails on all material delivered. This guarantee will be total on manufacturing and material breakage due to factory or manufacturing defects and/or failure to meet the operational requirements indicated in point 1.1.

## **3. VERIFICATION OF MATERIALS**

All materials will undergo tests and/or subsequent analyzes in the event that they present faults in the operation of the sails, during the period that they are under the supplier's guarantees.

The bidders must present suitability for the product offered, and an international ISO 9001 certificate. All bidding firms must have at least five (5) years in the market, dedicating themselves to the sale of sails for ships of more than THOUSAND TONS (1,000 Tn) of displacement, as well as you must also prove the provision of at least ONE (1) complete set of sails to large sailboats.

## **4. TESTING PROTOCOL**

After the provision of the material has been made and for the purpose of being able to complete the final reception, the sails will undergo the tests on board and at sea that are detailed below:

### **5.1. TESTS ON BOARD, IN PORT**

- a.- Verify that the sails do not present inconveniences or difficulties in their spanning.
- b.- Verify that the sails do not present visible flaws in the fabric.

### **5.2. SEA TRIALS**

- a.- Verify that the sails do not show wrinkles when inflated by the wind.
- b.- Verify that the sails do not present deformations on their surfaces.
- c.- Verify that the sails have good wind output.
- d.- Verify the resistance of the cuffs according to the existing wind table on board.
- and. - Check the resistance of the fists during turns.

**5. PLACE OF DELIVERY:** Where the Frigate School Ship A.R.A. "LIBERTAD", in the port of Baltimore (USA).

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6. **DEADLINE FOR DELIVERY OF THE GOODS:** The Successful Bidder must deliver the goods required in the place indicated above, between July 08 and 11 of the current year.

**OBSERVATIONS**

1. All requested material shall be new, unused and original, delivered in their packaging/box of the mentioned brands, as appropriate, perfectly packed and sealed.
2. All items must be delivered at the port of Baltimore, United States of America, during the vessel's between July 08 and 11, 2022.
3. All costs arising for the material final delivery on board onboard Frigate A.R.A. "Libertad" shall be borne by the supplier. All the above mentioned charges must be included in the unit price of each item of the quotation.

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**ANNEX III**

**REQUEST FOR QUOTATION FORM**

**File Nº: 200012**

**Private Bid Nº: 08 / 2022**

Offerer Information:	
Company Name:	
Address:	
State:	
Zip Code:	
Telephone number:	
Fax Number:	
Tax Identification Number:	
E-mail:	

**Signature.....**

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ITEM	UM	QTY	P/N	DESCRIPTION	QUALITY	UNIT PRICE	TOTAL PRICE	UNIT PRICE WITH DISCOUNT	TOTAL PRICE WITH DISCOUNT
1	EA	1	-	<b>Lower Top Stay Sail</b> (Luff: 23,00 mts; Foot: 9,90 mts; Leech: 17,50 mts; Diagonal/Radius: 7,00 mts).	FN	\$	\$	\$	\$
2	EA	1	-	<b>Upper Top Stay Sail</b> (Luff: 20,00 mts; Foot: 10,20 mts; Leech: 15,00 mts; Diagonal/Radius: 7,50 mts).	FN	\$	\$	\$	\$
3	EA	1	-	<b>Top Gallan Stay Sail</b> (Luff: 21,30 mts; Foot: 10,60 mts; Leech: 14,50 mts; Diagonal/Radius: 6,50 mts).	FN	\$	\$	\$	\$
4	EA	1	-	<b>Aft Gaff Sail</b> (Luff: 7,60 mts; Foot: 13,50 mts; Leech: 13,00 mts; Side: 9,00 mts, Diagonal/Radius: 15,50 – 14,25 mts).	FN	\$	\$	\$	\$
5	EA	1	-	<b>Lower Top Sail</b> (Luff: 23,00 mts; Foot: 24,70 mts; Side 5,90 mts).	FN	\$	\$	\$	\$
6	EA	1	-	<b>Lower Mizzen Top Stay Sail</b> (Luff: 23,00 mts; Foot: 9,90 mts; Leech: 17,50 mts; Diagonal/Radius: 7,00 mts).	FN	\$	\$	\$	\$

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7	EA	1	-	<b>Mizzen Gallan Stay Sail</b> (Luff: 21,30 mts; Foot: 10,60 mts; Leech: 14,50 mts; Diagonal/Radius: 6,50 mts).	FN	\$	\$	\$	\$
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**OBSERVATIONS**

1. All material quoted must include freight charges to the vessel's mooring location, licenses and any other needed expense. All the above mentioned charges must be included in the unit price of each item of the quotation.
2. As established in decree 1030/16 article 57 - variant offers - a variant offer is understood to be one that, by modifying the technical specifications previously provided in the terms and conditions bid package, offers a solution with an improvement that would not be possible to achieve in a case of strict compliance with the original bid package.

The jurisdiction or contracting entity will only evaluate and compare the base offer submitted by different bidders, and will only consider the variant offer from the bidder that submitted the most convenient base offer.

**PAYMENT TERMS:**     NET 90 DAYS

**DELIVERY LOCATION:** .....

**TOTAL AMOUNT (PRINT):** .....

**TOTAL AMOUNT WITH DISCOUNTS (PRINT):** .....

**Signature:** .....



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Las Malvinas son argentinas

**Hoja Adicional de Firmas**  
**Pliego Bases Condic. Part.**

**Número:**

**Referencia:** Pliego Ingles Req 200012

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